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Introduction

As stated in the previous chapter, fostering economic development continues to be a goal of this Comprehensive Land Use Plan. More year-round employment is paramount in order to slow or reverse the loss of talented youth taught by local schools, another goal of the community.

Equally important is an ability to maintain the quality of life and the assets that the Village enjoys. Alexandria Bay's physical access and visual access to the river, its natural beauty, its downtown and multitude of businesses, and the Village neighborhoods continue to make "ABay" a thriving place to live, work, and visit.

This Comprehensive Land Use Plan provides a general understanding of the factors that shape the development of the Village: the NYS Route 12/26 gateway to the Village and its distinct areas throughout including the waterfront areas, islands, downtown business area, near downtown and the

southeast residences as explored through the survey process.

Village Land Use

This section of the Comprehensive Land Use Plan is the Village's opportunity to make recommendations and raise considerations concerning the future quality, character, density and type of new development that may occur as a result of future infrastructure decisions and resulting development.

Under the "home rule" provisions of the New York State Constitution, municipalities, based on local Comprehensive Plans, have the authority to regulate land use through zoning and related regulations. This Comprehensive Plan is known as the basis for local land use considerations as well as capital improvements, infrastructure and zoning law amendments.

Alexandria Bay Vision and Goals

One of the most important yet challenging tasks facing the committee was to garner public input and draft a vision statement. The following Village Vision and Goals serve as broad mission statements and directions that Village residents thought were important to aim for and attain as part of its brighter future.

The vision and goals are tied to quality of life and land use planning as they relate to future development and municipal improvements. The subsequent strategies are more specific and address various components of each goal. Their development benefitted from public input, along with the plan inventory information

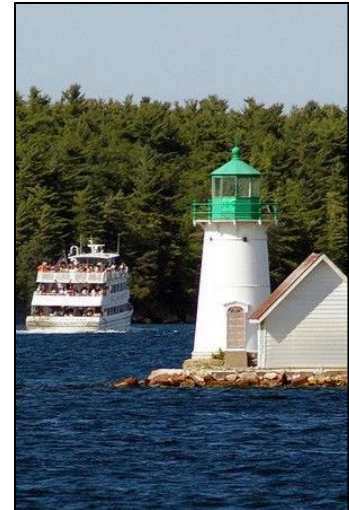
CHAPTER 8.

VILLAGE VISION, GOALS, STRATEGIES, AND CONSIDERATIONS

and guidance provided by the community, generated throughout the planning process.

process along with two public hearings (one held by the committee and one by the

This public input process consisted of a public input survey, an open house type meeting another public meeting later in the



Alexandria Bay's Vision

Alex Bay's residents, business owners, and community leaders wish to preserve the natural beauty and village character that has drawn visitors for over 200 years. Since the inception of the Village, the St. Lawrence River and many islands, bays, and harbors welcomed homesteaders, entrepreneurs and travelers. During the last 100 years residents, businesses, seasonal residents and tourists have continued to depend on the charm of this scenic small village interwoven with entrepreneurial spirit.

The unique combination of natural beauty, events, hospitality and Thousand Island tours including Boldt Castle provide world-class destinations welcoming tourists from around the globe. Its homes offer a variety of styles reflecting a rich classic heritage. Its businesses offer a variety of food, goods, social offerings and entertainment.

The undulating shorelines and open spaces that envelope the Village allow scenery and riverfront rural lifestyle to flourish, season after season, year after year. The Village prefers to build upon this success, enhance people's summer, shoulder season and year-round experiences and quality of life to continue to offer the world a place to visit, play, grow-up, live, return and ultimately retire as many do.

Village Board).

The goals and strategies were developed to be included in the Village's Comprehensive Land Use Plan in order to provide Alexandria Bay with direction and guide community enhancement well into the future.

In conclusion, the vision and goals served as the foundation from which the subsequent strategies of the Plan were devised. All elements of the strategies in the Plan should be developed, worked on and implemented in order to achieve elements of the vision and associated goals and strategies.

The Village hopes that the vision, goals, and strategies provide local leaders and developers with guidance to improve the Village's economy, character, streets and sidewalks, quality of life to sustain its role as the heart of the Thousand Islands overall economy. Alexandria Bay's ambition is that future development contributes toward the goals and strategies in an appreciable way to advance the Village and realize its vision.

Over time, implementing the goals and strategies will help the Village fulfill its vision. Doing so involves a number of strategies in order to keep the Village moving towards its goals.

- | | |
|-----------------|--|
| Goal #1: | <i>Foster economic development and family destination opportunities within the Village.</i> |
| Goal #2: | <i>Enhance functionality of pedestrian elements, crosswalk and sidewalk maintenance and safety in the Village.</i> |
| Goal #3: | <i>Guide land use development with context and site design considerations in order to protect and enhance Village character.</i> |
| Goal #4 | <i>Incentivize property and sidewalk maintenance in order to enhance pedestrian circulation to alleviate traffic/pedestrian conflicts.</i> |
| Goal #5 | <i>Increase year-round housing where feasible to enhance year-round market demand in the Village.</i> |
| Goal #6: | <i>Expand recreational facilities and/or programs to meet the varying needs of youth, young adults and older Village residents and visitors.</i> |
| Goal #7: | <i>Promote development types along the river and creek frontage that enhance visual or physical water access in order to foster investment.</i> |
| Goal #8 | <i>Pursue road modifications to enhance traffic flow, consider traffic calming measures and/or a shuttle service with satellite parking area outside the downtown.</i> |
| Goal #9 | <i>Consider adopting a complete streets policy to promote transportation enhancements and accommodations for all users: pedestrians, bicyclists, transit riders, and drivers of all ages, incomes, abilities and disabilities.</i> |
| Goal #10 | <i>Maintain and enhance the Village's mixed-use, historic and aesthetic character to capitalize on its historic structures and pedestrian friendly community.</i> |

In some cases, more cooperation will be needed, or additional work by a potential committee or existing board might be called

for. Still further, some additional tools may need to be developed to enable the Village in its capacity to effect desired change over time. Lastly, some private investment may be required to effect change where growth energy is desired to allow the Village Vision to be realized.

Goal #1: *Foster economic development and family destination opportunities within the Village.*

- | | |
|-------------|---|
| Strategy 1. | Broaden the spring and fall (shoulder) seasons to extend/lengthen summer tourism. |
| Strategy 2. | Reconnect and/or develop new winter recreational opportunities for all ages to enhance year-round business participation. For example, activities and events in the Bay such as ice fishing derbies, the polar dip, and Thousand Island Snowmobile Club activities or related chamber events. |
| Strategy 3. | Develop commercial and resort zoning designations to facilitate investment in tourism-based businesses to enhance access and uses near the waterfront. |
| Strategy 4. | Determine excess sewer and water capacity that could be marketed toward prospective family oriented or year-round businesses. |
| Strategy 5. | Consider zoning and code enforcement administrative services to streamline review process and/or standards for new and/or expanding businesses. |

Goal #2: *Enhance functionality of pedestrian elements, crosswalk and sidewalk maintenance and safety in the Village including traffic and parking elements to enhance safety.*

- | | |
|-------------|--|
| Strategy 6. | Work with Village DPW and local philanthropic organizations to explore pedestrian enhancement funding sources, sidewalk maintenance or cleanup days to work on enhancing pedestrian functionality throughout the Village. |
| Strategy 7. | Identify areas needing tree pruning, sidewalk construction or reconstruction to fill-in any gaps where pedestrians must now enter the street to use the sidewalk due to narrowness, physical barriers, or missing connections or crosswalks. |
| Strategy 8. | As witnessed in the summer months, many visitors and residents walk in and around the Village. For example, along NYS Route 12 pedestrians go to the Big M and other destination services. Advocate for construction of a pedestrian crossing at Church St/NYS Route 12 traffic light in the Village and a sidewalk along the south side toward the Big M to enhance safety and encourage walking vs the increased vehicular trips along this busy highway corridor. |

Goal #3: *Guide land use development with context and site design considerations in order to protect and enhance Village character.*

- Strategy 9. Development proposals within distinct neighborhoods should build upon historic building design elements and Village character such as proportion of building to the street or sidewalk, number of floors, rooflines, window patterns and rhythm, as well as building placement.
- Strategy 10. Natural physical features should be incorporated in the design of new projects. Inventory important natural physical features such as St. Lawrence River views, streams and important landscape elements such as rock outcroppings and implement such criteria while reviewing new projects.
- Strategy 11. Allow for higher density and larger scale development in locations they've been historically developed in the Village, and smaller scale with a limited mix of allowed uses where sensitive land uses or features exist.
- Strategy 12. Promote increased density to be pedestrian and bicycle friendly, and have a mixture of uses where appropriate. Require buildings to be located close to the sidewalk in these areas. Embed civic, institutional and commercial activity mixed within neighborhoods, not stand alone isolated structures or uses.
- Strategy 13. Establish criteria that ensures sites and services are within walking distance to decrease automobile dependency. This would include sidewalk connections, trees along the street, commercial uses with shallow setbacks oriented to the street, shared access to rear parking, residential uses oriented to Otter Creek and preserved views to the Creek.



- Goal #4** *Incentivize property and sidewalk maintenance in order to enhance pedestrian circulation to alleviate traffic/pedestrian conflicts.*
- Strategy 14. Clean up property blight and abandoned uses by removing deteriorated or degrading elements in the Village. Officials could meet with owners of dilapidated structures to discuss cooperation in renovation or removal.
- Strategy 15. Consider the merits of a Beautification Plan for the Downtown or selected neighborhoods by enforcing Clutter, Litter and Debris or property maintenance regulations with emphasis on the importance of the image of the Village to residents and visitors.
- Strategy 16. Consider simplification of sign and billboard regulations in the Village. Thus a review of sign standards for effectiveness and consistency with improving the overall image and character of the Village. Abandoned sign standards could be examined for maintenance or message updating needs.

- Goal #5** *Increase year-round housing where feasible to enhance year-round market demand in the Village.*
- Strategy 17. Examine the potential level of demand for year-round residential development in the Village by considering a residential market analysis using real estate figures, and local housing agency information.
- Strategy 18. Encourage diversity and design in housing types in the Zoning Law standards throughout the Village and along the St. Lawrence River resulting in affordable units for renters and home buyers.
- Strategy 19. Encourage in-fill housing developments with smaller lot or clustering techniques and allow for access easements to the St. Lawrence River or Otter Creek. Allow moderate density residential development such as condominiums in these areas to encourage growth.
- Strategy 20. Work with housing agencies and local banks to encourage improving and maintaining the local housing stock and possible adaptive reuse of existing structures.
- Strategy 21. Study the need for efficient municipal water and sewer infrastructure improvements in order to expand capacity to accommodate new growth.

Goal #6: *Expand recreational facilities and/or programs to meet the varying needs of youth, young adults and older Village residents and visitors.*

- Strategy 22. Work with recreation or youth committee to look at recreation development needs including potential need for more ball fields, trails, public water access for swimming, fishing access and/or boat launches. Look at winter time recreational activities such as sledding, snowshoeing, cross country skiing, outdoor skating or other options.
- Strategy 23. Work with the local Snowmobile Club to identify service needs and possible linkages to the trail to connect into the Village. Look at ways to make sections of the trail available for year round recreational use.
- Strategy 24. Consider a feasibility study for construction/renovation of an ice arena. The Village recognizes the financial impact of hockey and figure skating competitions have on the immediate area and beyond especially for enhancing year-round appeal for residents and visitors.
- Strategy 25. Consider options for a recreation center that includes indoor ball fields and other facilities to enhance year-round recreational opportunities for the Village and surrounding Towns.

Goal #7: *Promote development types along the river and creek frontage that enhance visual or physical water access in order to foster investment.*

- Strategy 26. Consider structural placement to retain views, have access to the river or creek, and provide visual organization to a development.
- Strategy 27. Make efficient use of the land along the waterfront with in-fill development guidelines in the Subdivision Law and by planning roads that are set back from the River.
- Strategy 28. Amend site design preferences in the site plan review criteria in the Zoning Law for aesthetic impacts such as screening of parking, architectural character, and driveway length. Ensure new development in the Village has attractive design elements.
- Strategy 29. Review Zoning Law to consider land use types to encourage and discourage within each zoning district. Include lot size changes that further develop the goal of allowing infill and encouraging denser development in certain districts and less dense where larger lots predominate.

Goal #8: *Pursue road modifications to enhance traffic flow or consider traffic calming measures.*

- Strategy 30. For example, mitigate bottlenecks and hazardous crossings, thereby reduce vehicle and pedestrian conflicts and enhance safety in and around downtown.
- Strategy 31. Consider bump-outs to shorten crosswalks, which will enhance pedestrian safety, calm traffic and increase driver\walker visibility near intersections.
- Strategy 32. Pursue\consider the need for summer delivery (off peak) hours in the downtown area, which would enhance traffic flow during peak traffic hours.
- Strategy 33. Discuss and explore options for a shuttle service throughout the Village with satellite parking area during major events as a pilot project or trial run, to gauge interest and potential user demand.

Goal #9 *Consider enhancing the Village's; street, sidewalk, crosswalk, and parking design to promote transportation enhancements and accommodations for all users: pedestrians, bicyclists, transit riders, and drivers of all ages, incomes, abilities and disabilities.*

- Strategy 34. Use complete streets criteria when conducting project reviews to address transportation for all users; pedestrians, bicyclists, potential transit riders, and drivers of all ages, incomes, abilities and disabilities.
- Strategy 35. Pursue funding sources for street and sidewalk improvements (in phases if needed) to enhance transportation\pedestrian safety and efficiency for all users.

Goal #10 *Maintain and enhance the Village's mixed-use, historic and aesthetic character to capitalize on its historic structures and pedestrian oriented community fabric.*

- Strategy 36. Consider developing guidelines (standards and sketches) for new and redeveloped buildings to complement the historic business, mixed use and residential forms common to the Village to reinforce its historic and aesthetic character. Standards should apply to pedestrian access, signage and franchise/chain businesses as well.
- Strategy 37. Improve the overall aesthetics of the Village by addressing vacant or dilapidated buildings, unscreened dumpsters or clutter, uninviting building facades, cluttered or obstructed sidewalks, and abandoned signage.

Recreational basis and assumptions: The Village recognizes that having recreation facilities provide year round activities for visitors and residents alike would enhance its quality of life. Access to recreation is especially important for families with children and an aging population. As the desire for Village quality of life options becomes increasingly important, there will be a growing need for more recreational opportunities to meet residents' needs.

As a region with a significant tourist economy based on its natural beauty and destinations, there are opportunities for the Village to build upon its recreational resources as an economic development tool. While the St. Lawrence River is the most heavily used recreation "playground" in the area, there are other opportunities for recreation that would help expand the tourism season and enhance the area's appeal to potential new residents and residents to stay in the area.

Village Land Use and Form Considerations

Land uses appropriate for the Village include single family residences, two-family residences, small scale mixed use buildings (residential/commercial), multi-family in strategic areas, institutional uses and small scale retail commercial or office buildings. Village land uses near downtown land uses include single family and occasional multi-family residences in well-defined neighborhoods that offer a diversity of housing choices often within a short walking distance to the downtown or business areas as well as other recreational and cultural amenities. Homes located along primary streets and side streets, are often connected with a sidewalk system along the street. Building setbacks from the right of way are

shallow and mostly uniform, with relatively narrow lots that place homes in close proximity to each other. Residential streets in these areas are of a width that promotes slower vehicular speeds. Residences are the primary land use within the character area. Schools, small scale retail and offices, parks, and churches occur as well and are accessible by vehicles and pedestrians (i.e. uses in close proximity with pedestrian walkways).

Villages serve as destinations and activity nodes for outlying areas. New development should be pedestrian friendly in both design, layout and development. A primary driver of character within the village area is the relationship between buildings and the street. Consistency in height and massing are important to the sense of place. The increased density and height creates distinct opportunities and challenges for communities striving to grow and revitalize. A critical design consideration for the Village character area is density of housing units. Villages typically have a high density unit-per acre. However, it must retain its small town character. Another crucial design consideration is the connectivity of neighborhoods to public spaces. Village areas often have small lot sizes that prohibit expansive areas for relaxation and recreation. As a result, there is often a demand for larger open spaces and programmatic options for its residents. Centralized public open spaces vary in size and provide opportunities for structured play or passive recreation. Such recreation spaces must have direct pedestrian and/or street level connections.

Important Alexandria Bay Village design priorities:

- Retain uniform setbacks along main street frontages to maintain pedestrian friendly design.
- Reduce or eliminate “saw-tooth” building placement on lots created by parcels with deep setbacks\vacancies next to shallow ones. Consider build-to line standard.
- Retain consistent building heights along a street frontage, with no more than 2.5 to 4 stories high or similar to surrounding building heights.
- Encourage similar uses on opposite sides of the street or road.
- Parking lots should be located on the side or rear to maintain close proximity of the building to the street.
- Orient principle entrances to primary streets.
- Connect entrances to sidewalks (if present).
- Construction materials should be consistent with traditional building materials or appear so.
- Architectural character should emulate traditional or vernacular styles within the area.
- New development should include significant landscaping (street trees).
- Retain specimen and/or mature trees, especially those along the roadside.

- Utilize only native and/or hearty species that will survive in the North Country.

Introduction-Planning Project Considerations

The following Planning Project Considerations should be considered for use when reviewing development or redevelopment projects, updating the zoning regulation review criteria used to review projects, and establishing subdivision requirements which establish minimum standards for lot creation, road design, and ultimately, the pattern of development for generations. They are the product of discussion and thought about how development can respect the area and be shaped to complement the character of the Village and ultimately improve the quality of life for current and future residents.

Village Planning Project Considerations - overall

- ✓ **Future growth potential** - explore funding opportunities to expand sewer and water capacity. Identify appropriate areas within the Village for suitable population and/or employment growth.
- ✓ **Attracting growth** - foster appropriate development and infill opportunities that interconnect with existing neighborhoods and business areas that improve the overall desirability and destination quality of the Village.

- ✓ **Curb cut/access management** - shared driveway accesses and internal access connections among adjacent businesses are favored over excessive numbers of individual curb cuts with no connections.
- ✓ **Connecting current businesses** - as new projects occur, foster traffic and pedestrian connections to existing business and residential areas and allow improvements that will improve existing business competitiveness, alleviate congestion, and enhance aesthetics.
- ✓ **Drainage affects water quality** - drainage facilities should be incorporated onsite and existing drainage systems should be improved/upgraded or maintained to limit storm water impacts downstream or on neighboring properties. Such drainage facilities should include detention and retention, bank stabilization, and safe practices for snow removal and lawn care to keep particulates and contaminants from draining into local water bodies.

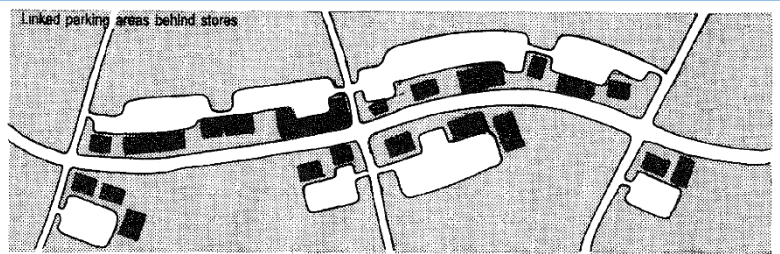
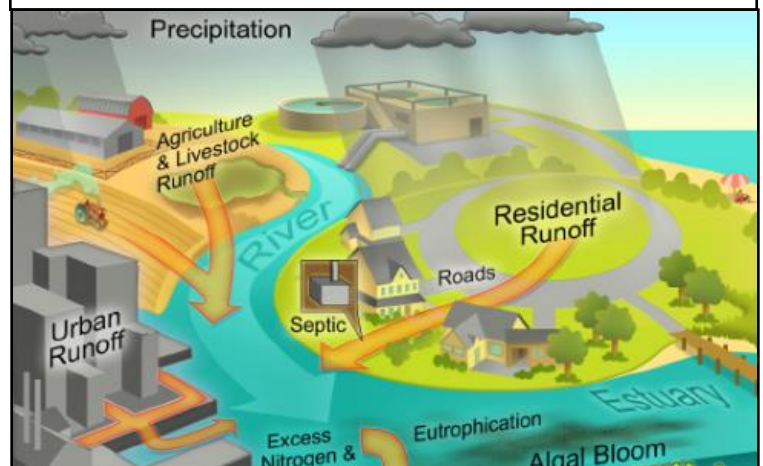


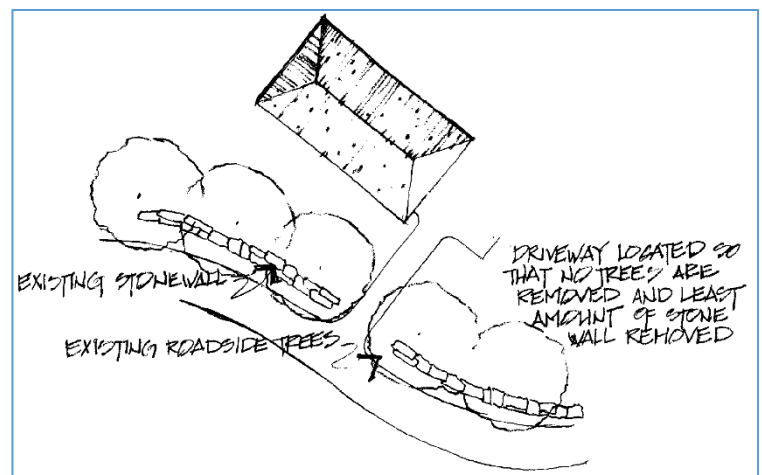
Figure 9-22. Connecting rear parking lots allows customers to drive to many other shops in the corridor without re-entering the highway and interrupting traffic flow. Such arrangements can be required for new development, expansion of existing buildings, and redevelopment.

Surface water flow example: illustrates common pathways that contaminants can be transported by surface water flow



For example, any substance within the watershed which can be transported by water (e.g. detergents, eroded soil, septic effluent, pesticides, & oil/road dust) can eventually reach the river, stream or lake and affect water quality. It is not only shorelines uses, but activities anywhere within a lake or stream's watershed which affect water quality.

- ✓ **Existing features** – where existing character features occur such as roadside trees, stone walls, tree lines, fencerows (which often have trees and fences of some kind), they should be preserved (or disturbed as little as possible). Such features serve to retain the rural character of roads.



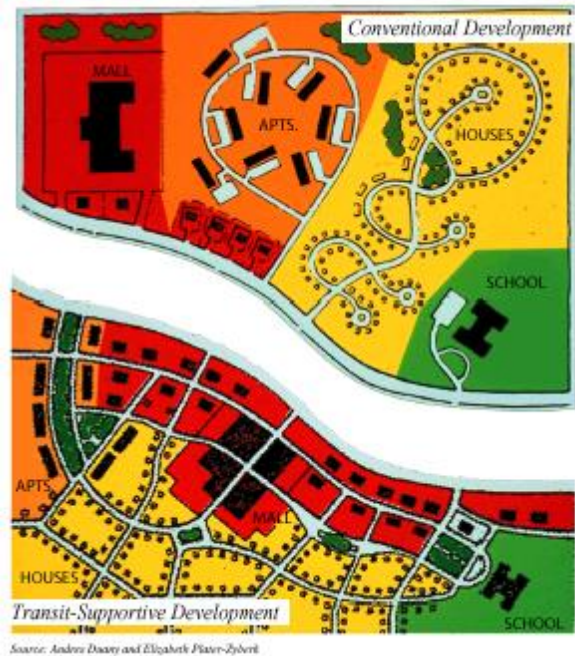
- ✓ **Historic character street layout** - consider requiring new development areas to extend the grid pattern with blocks and multiple connections to maintain traffic flow and access.

For example, a lack of a grid pattern can funnel too much traffic from a side road onto the main traffic artery which often creates a congestion point. However, extending the grid pattern allows flow from several side roads at once, which typically softens the overall traffic impact.

- ✓ **Historic building form & styles** - Where appropriate, consider guidelines for historic compatibility when new developments are proposed and when reuse of existing buildings and homes occurs.

For example, within historic districts or areas with a predominant style, form or scale, new structures should be required that echo the scale, style, form, rhythm and character of the neighborhood. Avoid placing a one story building in a three story block or a concrete-sided building on a street of wooden sided buildings. Consider consistency with size and materials whenever a new building is proposed, or when renovations that could affect the appearance of an existing structure are proposed.

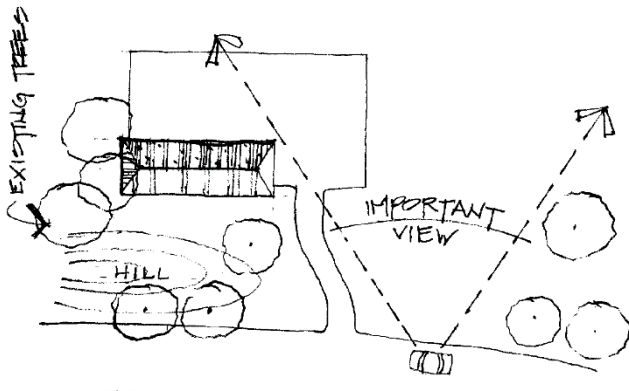
- ✓ **Mixed use buildings and projects** - Compatibility could include mixed use developments where feasible to include the historic pattern of services/employment centers with residences above or nearby, to allow enhanced pedestrian opportunities and decreased traffic congestion.



For example, interconnected mixed-use projects allow pedestrians to live and walk to nearby work and entertainment opportunities or to needed goods and services without having to drive distances to do so. This limits traffic congestion and parking demand.



AN EXAMPLE OF A BUILDING OUT OF SCALE AND CHARACTER WITH THE EXISTING BUILDINGS ON EITHER SIDE.



BUILDING LOCATED OUT OF THE WAY OF A BEAUTIFUL VIEW OF THE COUNTRYSIDE, TAKING ADVANTAGE OF A SMALL HILL AND EXISTING TREES TO SCREEN THE BUILDING.

- ✓ **Building placement** – Buildings should be sited so that obstruction of important or priority views from roadways, sidewalks, and parks will be minimized. This can be achieved by taking advantage of topographic changes or existing vegetation.
- ✓ **Underground Utilities** – Utilities for new development should consider the benefits and feasibility of underground placement. Not only do they improve property values with improved aesthetics (removal of unsightly poles and wires), they also provide improved reliability during severe weather, resulting in far fewer power interruptions, have fewer motor vehicle accidents, and reduce live-wire contact injuries.

✓ **Green Infrastructure** – Rain gardens or bioswales can be included in medians and along parking lot perimeters. Benefits of which include less water runoff, heat island mitigation, and a more walkable and pedestrian friendly environment. Permeable pavements also reduce runoff most of the year.



Rain Garden (also known as a bioswale) captures stormwater as it flows down the

Project Considerations - Residential

- ✓ **Building setbacks vs build-to lines** - within the Village, setbacks should be set depending on the prevalent pattern in the area, in some cases shallower build-to lines that maintain the small historic front yard pattern with larger rear yards should be required to maintain historic residential and business patterns close to the street. In less dense areas where primary buildings are

further from the road, larger setbacks could be maintained.

- ✓ **Highway frontage development, vs new roads/streets** - strip development should be discouraged where possible, to maintain traffic carrying capacity of arterial and collector streets. Therefore, new streets or local streets should be used for new development where feasible.

For example, lots that are created one at a time along a main highway can slowly alter the function of the arterial road. As each fronting lot creates a subsequent driveway access, it allows another potential conflict point and reason that traffic must slow down or face either an oncoming automobile or exiting vehicle that may be decelerating or accelerating which ultimately affects traffic flow.

- ✓ **Clustering** – clustering of businesses or homes should be encouraged where feasible to limit strip development and allow open space character and farming activities to be preserved while desired growth occurs.

Placing homes near one another also minimizes the installation and maintenance costs of running roads and utilities to them, and allows a better sense of community.

- ✓ **Pedestrian scale or walkable to\from – where feasible**, foster walkable projects that include sidewalks and pedestrian paths, within walking distance from other destinations, and are in scale with village businesses and residential areas.

For example, pedestrian scale typically balances pedestrian and vehicular needs while providing comfortable environments for people to assemble and associate with others. Community

design should be human-scale with services within reasonable distance from one another.

The following walkability standards are recommended: homes within ¼ to ½ mile of most services; elementary schools within ¼ to ½ mile of homes; parks within an eighth to ¼ mile of homes; downtowns should provide a balance of retail and commercial stores and services, e.g., hair salon, hardware store, pharmacy, grocery/deli, restaurants, clothing, post office, library, town\illage offices within ¼ to ½ mile of the community center. Areas not being used by pedestrians should be assessed to determine possible reasons for lack of use.

- ✓ **Soil Conditions influencing development patterns** - based on existing soils, ensure projects address individual septic and drainage issues to limit contamination and off-site impacts.

- ✓ **Dead-end streets vs loop streets** - dead end streets should only be used to access a limited number of homes (less than twenty), after which a second connection should be provided to an arterial or collector road.

For example, if the single access became blocked by an accident or incident and an emergency occurred in a subsequent house further up the single access road, getting to the 2nd emergency

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could be delayed or even blocked off entirely for a period of time.

- ✓ **Village lot sizes, smaller vs larger** - where feasible, smaller lots should be required to maintain the historic residential and business density with housing and business patterns close to the street to maintain pedestrian scale development.
- ✓ **Cost effective services** – Infill projects should be encouraged where services exist, or where possible, municipal services should be laid out in a compact manner to limit future maintenance costs.
- ✓ **Connections between housing areas** – Residential developments should be connected by internal road to limit trips onto the main traffic artery, also shared driveway accesses are favored over an excessive number of curb cuts. Refer to the curb cut/access management image.
- ✓ **Preserve open space/sensitive lands** - Open space and sensitive lands can be preserved by requiring the project to identify and set aside such areas and allowing smaller house lots in those cases. This improved layout often leads to a more marketable project, with open space areas and trails often that can be shared by the residents.

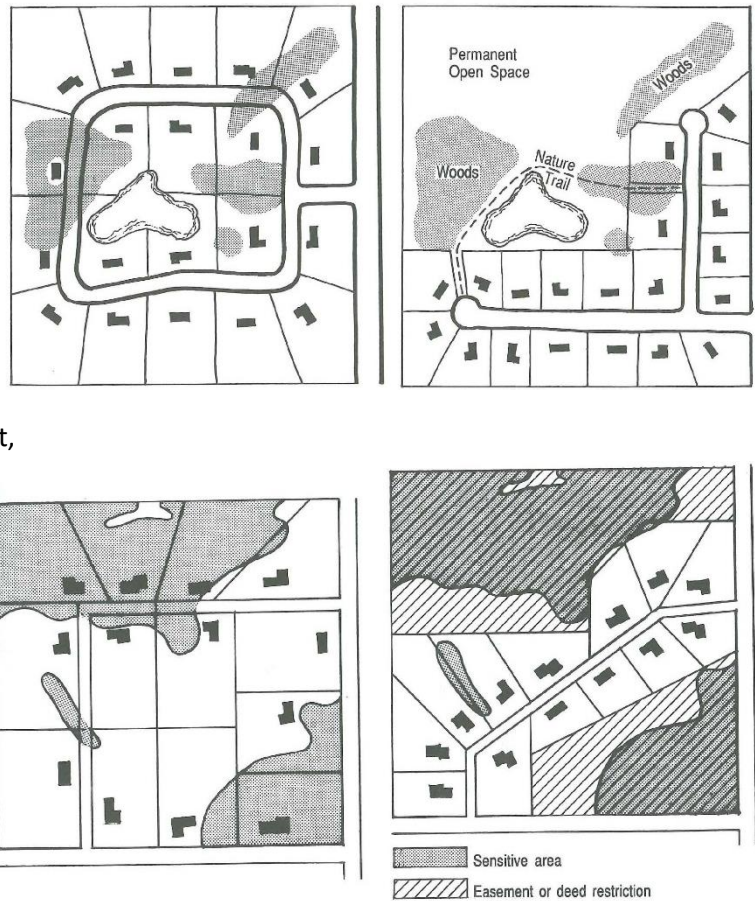


Figure 2-13. Conventional two-acre lot subdivision with homes located on sensitive but buildable land, compared with improved layouts protecting those resource areas, as encouraged by new regulations adopted by the Maine Department of Environmental Protection.

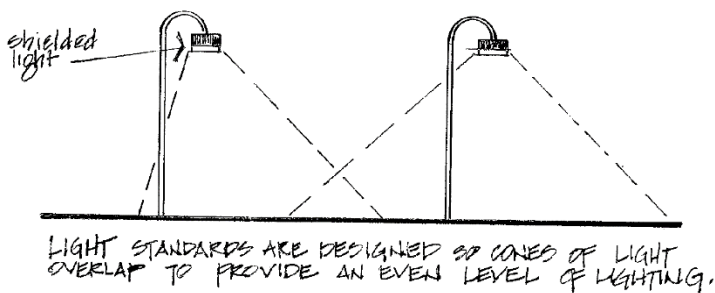
Project Considerations

- Commercial

- ✓ **Connections between parking areas/developments & shared access** - developments should be connected by street access or parking lot connectivity to limit trips onto the main traffic artery, also shared driveway accesses are favored over an excessive number of curb cuts.

- ✓ **Lighting** – Lighting should be used where appropriate, however, over-lighting and excess glare should be avoided, especially on neighboring properties and the public roads. Shielded or cutoff lights should be used to minimize lighting spill-over.

For example, lighting should be controlled in both height and intensity to maintain rural character. Light levels at the lot line should not exceed 0.2 foot-candles, measured at ground level. To achieve this, light fixtures should be fully shielded to prevent light shining beyond the lot lines onto neighboring properties or roadways.



- ✓ **Building setbacks** – Maintain current setbacks in business areas utilizing build-to lines.

For example, setbacks often push new buildings away from roads, fostering a contrasting character and anti-pedestrian pattern than historical patterns of development. Build-to lines require buildings to be placed closer to the street, allow parking to the side and rear, and create a pedestrian friendly streetscape.

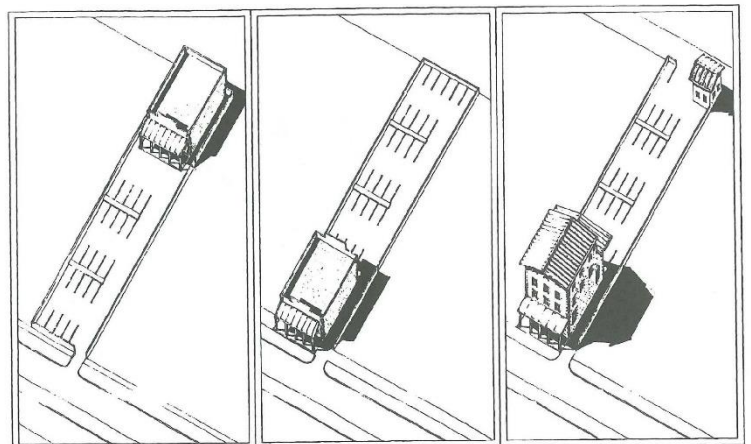


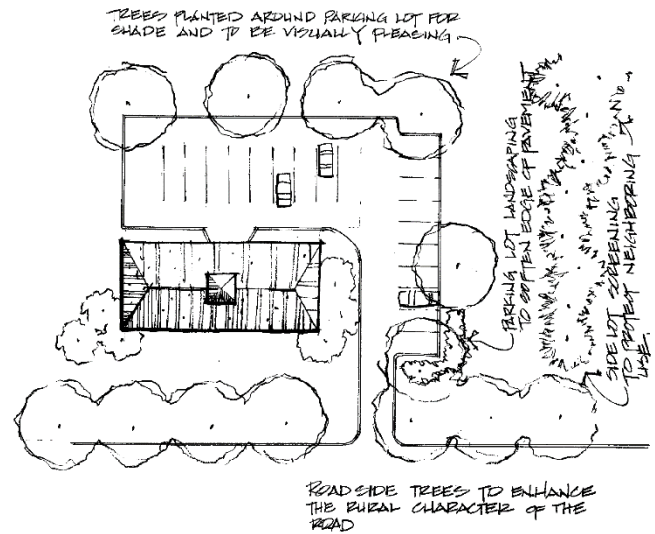
Figure 9-10. Alternative locations for buildings, parking, and access, Davie Settlement, Broward County, Florida. Source: Dover et al., 1990.

- ✓ **Infill** refers to the development of vacant parcels within previously built areas. These areas are typically served by public infrastructure, such as transportation, water, wastewater, and other utilities.

- ✓ **Parking to the side or rear** – the bulk of parking areas should be smaller, distinct areas to the side or rear to allow closer building placement to the road in order to maintain community character, reinforce the visual presence of building as opposed to parked vehicles and the pattern of buildings along the roadside.
- ✓ **Landscaping** – appropriate landscaped buffering should be used to soften parking area edges and buildings, including screening views between uses where needed and partially screen views of parking areas from public roads.

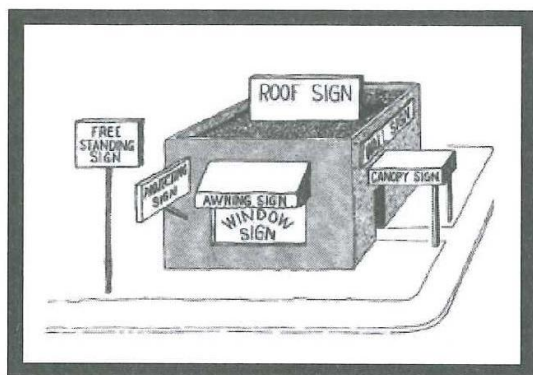


- ✓ **Mixed use development** – mixed use developments should be considered where feasible to include the historic pattern of services\employment centers with residences above or nearby, to allow enhanced pedestrian opportunities and decrease traffic congestion.
- ✓ **Pedestrian scale or walkable to\from** - foster walkable projects with buildings near the street that include sidewalks or pedestrian paths, are within walking distance from other destinations, and are in scale with Hamlet businesses and residential areas. See above description of pedestrian scale in the overall considerations.



- ✓ **Business hours of operation** - for offices or businesses locating near or within primarily residential areas, consider compatible hours of operation (including hours that parking area lights are used).
- ✓ **Maximum building heights** – consider building heights compatible with current Village business and residential buildings to maintain historic patterns and community character.
- ✓ **Signage** – Where appropriate, adequately sized signs should be used (consider letter size and total sign size based upon the speed limit), with a total size allowable limit to ensure efficient signage. Within Village areas especially with slower speed limit zones, smaller, lower, and externally lit signs should be used. Free standing signs should consider lower monument style. Internally lit signs should be constructed to limit glare. Glare from all signage should be minimized. Except for directional signage, limiting off premise signs along the Seaway Trail Scenic Byway could be considered.

For example, Saratoga Springs, New York, regulates freestanding signage based on speed limit: downtown area is limited to 12 feet in height, 12 square feet in area; other districts within areas of slower speeds such as those 44 mph and less, 12 feet in height and 24 square feet; district areas with speed limits of 45 mph or greater, 20 feet in height and 40 square feet in size.



Source: *Signage Made Simple* - Monmouth, NJ County Planning

Alternative Energy Project Considerations

As sustainable energy sources become more prevalent and offer options for local energy production, local priorities for such alternative energy projects should be established. Similar to the above Planning Project Considerations, the following considerations should be used when updating the zoning criteria used to set standards for their review. Alternative energy systems may have an impact on adjacent properties or neighborhood aesthetics, therefore municipalities should review their land use regulations to facilitate opportunities for promoting renewable

energy in a way that reflects community values and planning.

Such solar, wind turbine, geothermal, and biomass requirements should address potential impacts to protect the community, its long-term quality of life, and economic value. Therefore, adequate standards should be put in place to allow the Town maintain its local priorities.

✓ **Solar Energy** - Solar panels that create electricity from sunlight or use its energy to heat water can be placed on residential rooftops, accessory buildings, or installed as free-standing, ground-mounted structures. As solar energy systems may have impacts on adjacent properties or neighborhoods, local solar energy standards should address small (on-site electricity use) and large scale (electricity sold to the grid) facilities.

✓ **Wind Turbines** – Wind turbines that create electricity from wind can be constructed for different purposes: onsite use (residential, small business, or farm), municipal or commercial. As wind turbines can have impacts on adjacent areas, neighborhoods, and the community at large, local turbine standards should address small and large scale projects as community priorities become established.

✓ **Geothermal** – While resources vary from location to location, new technologies have emerged that are capable of utilizing lower temperatures. Geothermal power can also be harnessed for low intensity heat at relatively shallow depths, which can be used for space and water heating and cooling. As

geothermal systems may have impacts on adjacent properties or neighborhoods, local geothermal standards should address small and large scale facilities.

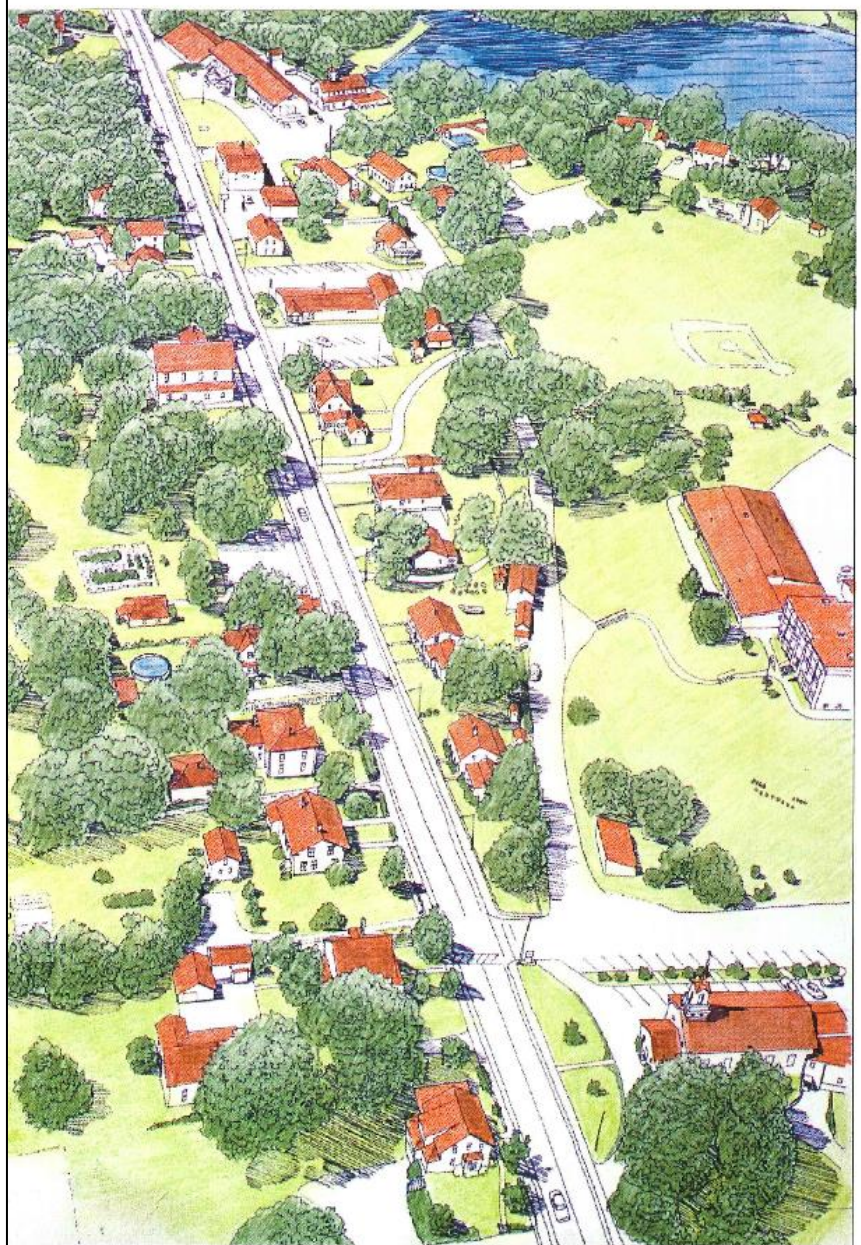
✓ **Biomass** – Biomass refers to organic matter that has stored energy through the process of photosynthesis. Such projects can take a variety of forms: electric generation, gasification, anaerobic digestion, or biofuels. Biomass projects can have impacts related to local air quality, water supplies, wastewater disposal, outdoor lighting, facility size, steam emission, noise levels, and traffic levels reinforcing the need for local standards.

Infill Development Forms: Conventional and Creative

The following images illustrate development scenarios based on conventional vs creative forms. They illustrate an area before infill development, then the same area with conventional infill development, and that area with a creative form of infill development.

This example demonstrates a mix of residential, commercial & institutional uses in an Historic Village Center infill development scenario from a publication: *South County Design Manual: South County Watersheds Technical Planning Assistance Project - 2001.*

Historic village center - prior to infill development



Historic homes, commercial buildings, brick mills, churches, and other buildings along Main St. Variety in size, shape and architectural styles, unified by the scale and function of Main St.

New gas station /convenience store follows corporate site plan, breaking the street wall by setting the building toward the rear of the lot. Bright colors and corporate logos dominate the canopy, brilliant lights at night move even further from local tradition.

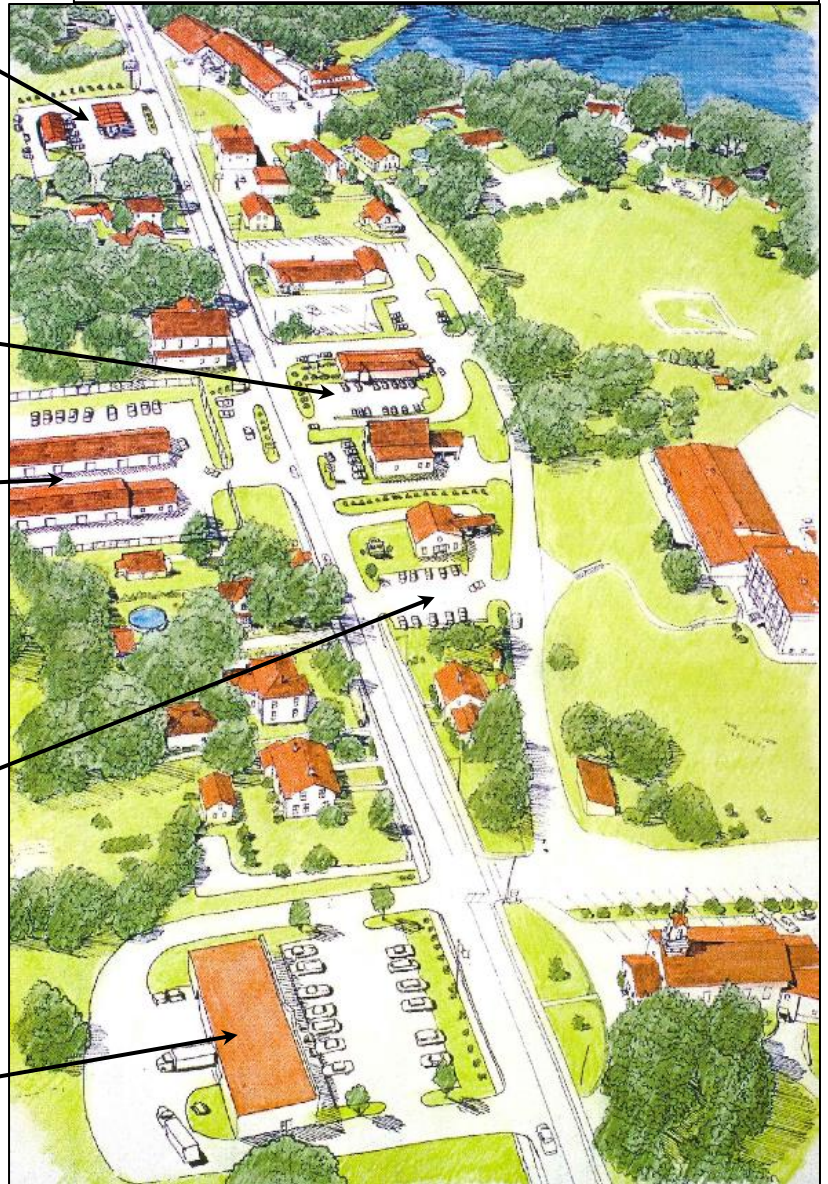
Replacement of historic home with doughnut shop removes an element that was unique to Village in favor of standardized building, parking lot, and drive-through.

Storage units in the center of the village replace active uses with functional dead zone. Since most access with need to be by car and truck, little reason for interaction with surrounding uses. Structures tend to be long, flat, and out of scale with the neighborhood.

Conversion of existing buildings from residential to commercial use. Ad hoc redevelopment of individual lots and excessive parking requirements promote inefficient use of space and over-paving.

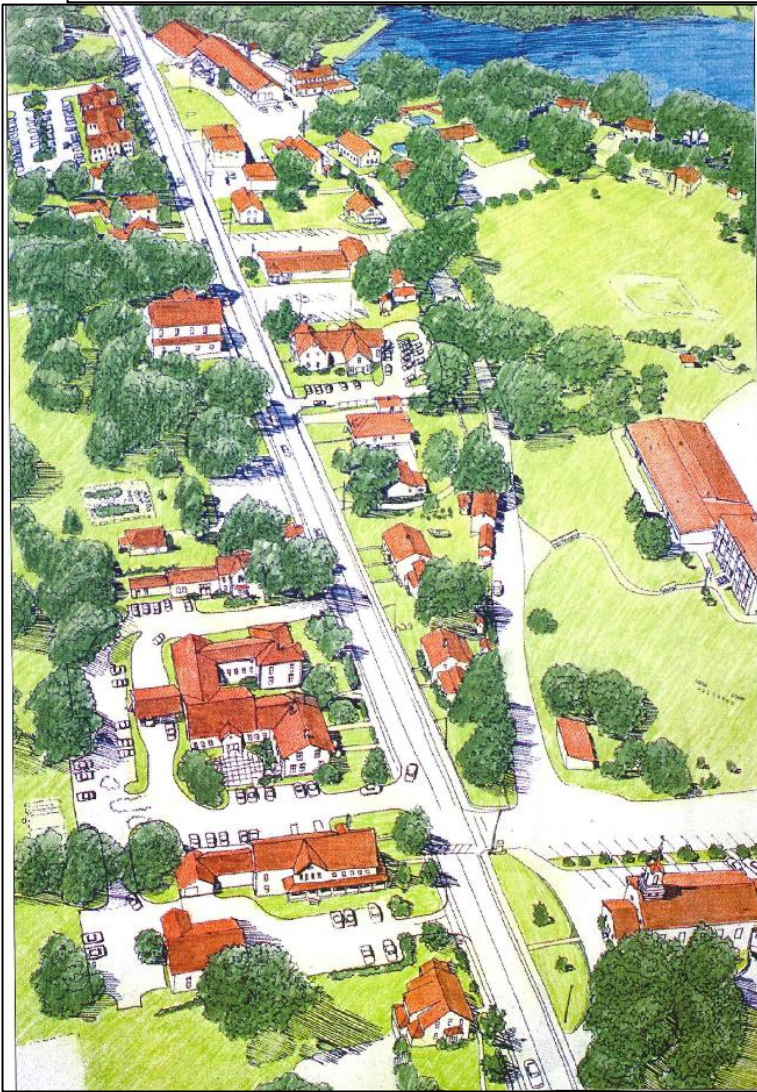
Historic homes replaced with strip mall. Typical design approach calls for parking in front, service access in rear, two curb cuts for ease of vehicle access. The result is a design that is very convenient for people in cars, but isolated from the surrounding community by asphalt.

Aerial view of Historic Village Center - after potential Conventional Infill Mixed Development



Functionally, it is still a 19th century village, with homes, a school, churches, commercial and government uses in close proximity. Currently is a walkable community, with a high degree of livability and sense of place.

Aerial view of Historic Village Center - potential Creative Infill Mixed Development



Design concept: New uses are required to conform to the visual character and physical patterns of the existing village, rather than the other way around. The emphasis is on maintaining a compact, pedestrian-friendly environment, while meeting the demands of the marketplace for convenient vehicular access and parking.

Land Uses: A mix of residential, commercial, institutional uses would be encouraged, with an emphasis on smaller scale businesses that could fit into existing buildings or new buildings at a compatible scale.

Access: shared curb-cuts between parcels reduce conflicts between cars and pedestrians and improve the streetscape appearance. Driveway connections cross lot lines, which minimizes the number of curb-cuts and allows customers to visit neighboring businesses without pulling back onto Main Street. Drive-thru windows and associated queuing lanes at the rear of buildings allow a needed function, for many modern businesses, while keeping the streetscape pedestrian friendly.

Parking: Parking lots at the side and rear of structures break areas of asphalt up into smaller units more in scale with existing structures. Plantings can also separate areas to keep asphalt to a minimum.

Architecture: Existing buildings are retained, with additions placed to the rear in compatible architectural styles. Larger uses are accommodated by connecting existing buildings together.

Landscape: Shade trees would be added, and existing trees preserved which would shade new parking lots and reduce their apparent scale. Evergreen shrub plantings screen parking from view.

Streetscape: Improvements to and maintaining sidewalks, addition of benches and trash receptacles, and pedestrian-scale

street lights encourage people to walk to and between uses. Existing utilities could be buried, improving the appearance of the street, opening up views of historic structures and preserving trees.

Pedestrian enhancements: Sidewalks renovated to provide easy pedestrian access to surrounding parcels. Landscaping helps to screen parking: design and materials follow shared Main Street landscaping guidelines.

However, under current zoning in this example (Historic Village center), lot size and setback requirements, minimum areas for parking, and open space requirements for each building lot make it hard to build anything without tearing down existing buildings and consolidating lots. While this had slowed development to some extent, typically it's only a matter of time before the rewards to develop outweigh the costs of pursuing this inefficient style of development.

It also means that development is also likely to be driven, not by local residents, but by corporations in some cases looking to expand franchise coverage. The result often does not relate to the existing village in either scale or appearance, tends to favor automobile over pedestrians (which erodes walkability), and which almost ensures the loss of historic character and architecture that remains in the village.

Village Recommended Coordination and Implementation Steps

Implementing the Comprehensive Plan requires a series of Village Board policy decisions, program initiatives and local, county and/or state or regional agencies. Some actions recommended in the Plan will also require funding.

This Plan details a series of diverse action steps including potential capital improvements, administrative actions, zoning amendments and potential policies and programs in some cases. Successful implementation of these actions over the next 5 to 10 years may depend on setting priorities and allocating limited resources – to the most important projects.

The Village Board of Trustees will oversee all aspects of implementation of the Plan and will direct work to be done in the future. However, other groups and agencies should be called upon to assist the Board when needed including the Planning Board, Zoning Board of Appeals, Village Zoning Officer, other local officials, organizations and potential new committees as needed along with Village Trustees.

The Village should ensure that copies of this plan are available at the Village Office, in the local library and on the municipal website once finalized including the maps and any appendices.

CHAPTER 8.

VILLAGE VISION, GOALS, STRATEGIES, AND CONSIDERATIONS

The Village Board should create/appoint members to a Comprehensive Plan Implementation Committee or other committees as needed to move the priority actions recommended in the Plan forward. The committee (s) should be comprised of a diverse group of representatives (similar to the Comprehensive Plan Committee) of elected and appointed officials, citizens, business owners and landowners to help guide implementation of the plan. The Village Board should clarify its expectations, time-frames, along with any funding or other assistance such as the County Planning Department staff that it may choose to make available for the committee's implementation efforts.

One suggestion would be to include at least some of the members of the Comprehensive Plan Committee to help the implementation process benefit from a clear or at least familiar understanding of the intent of the Plan as the Planning Process unfolded.

Updating the Plan should be considered every five to ten years, or beyond that time period if changes have occurred in the community then the Plan may not reflect current priorities.

Further, the Village should review the list of priority actions being implemented on an annual or semi-annual basis to determine the status of efforts, as well as to identify other actions that should be implemented next. At its annual reorganizational meeting, or shortly after, the Village should review the status of implementation of the Plan, and ask for reports from the Zoning Officer,

Planning Board and Zoning Board of Appeals regarding any concerns or issues have arisen over the past year that may need to be addressed in the Plan, zoning regulations or other programs in the Village. This work session could also be devoted to reviewing the action plan, identifying goals reached, retiring completed ones from the action list and adding new actions to be implemented if applicable.

The Village Board should use the following table below to help guide its implementation efforts. Some actions are short term and low cost. Others are medium terms while others are long term that may require more time or funding to implement.

The following actions are identified as priorities for the Village to begin work on soon after Comprehensive Land Use Plan adoption.

CHAPTER 8.

VILLAGE VISION, GOALS, STRATEGIES, AND CONSIDERATIONS

VILLAGE OF ALEXANDRIA BAY COMPREHENSIVE PLAN IMPLEMENTATION TABLE

RECOMMENDED ACTIONS: (in no particular order, except by priority type)

Check when done	<u>SHORT TERM PRIORITY ACTIONS (within 1- 2 years after plan adoption)</u>		Action	LEAD RESPONSIBILITY	CRITERIA FOR EVALUATING SUCCESS
	ACTION STEP A	Review and revise existing zoning law to simplify formatting and clarify where Village intent may not be clear.	Zoning Text amendment	Village Board of Trustees upon recommendation of Committee	Law adopted by Village Board
	ACTION STEP B	Update and add definitions within the zoning law to include any current, expected or desired compatible uses.	Zoning Text amendment	Village Board of Trustees upon recommendation of Committee	Law adopted by Village Board
	ACTION STEP C	Update signage requirements within the zoning law to reflect historic and modern signage while enhancing Village character.	Zoning Text amendment	Village Board of Trustees upon recommendation of Committee	Law adopted by Village Board
	ACTION STEP D	Update the zoning law to protect existing land uses and encourage compatible development in scale, type and character to enhance the community.	Zoning Text amendment	Village Board of Trustees upon recommendation of Committee	Law adopted by Village Board
	ACTION STEP E	Update the zoning law to enhance pedestrian access along the street and among adjacent properties, to encourage complete street policies throughout the Village	Committee or Planning Board	Village Board of Trustees upon recommendation of Committee	Criteria adopted by Village Board
	ACTION STEP F	Enhance the Village gateway with streetscape landscaping	Planting of annual flowers and/or shrubs	Village DPW	Plants flourish seasonally
	ACTION STEP G	Future zoning law applications that include site plans incorporate stamped drawings prepared by an engineer or architect.	Zoning Text amendment	Village Board of Trustees upon recommendation of Committee	Law adopted by Village Board
	ACTION STEP H	Consider a shared parking lot program which could promote public parking lot use for longer term shoppers and employees, thus using on-street spaces for short term customers	Parking lot plan and signage	Village Board of Trustees upon report of Committee	Direction signage and striping
	ACTION STEP I	Consider requiring green infrastructure stormwater management where flooding should be controlled to minimize natural waterway runoff impacts and localized flooding.	Committee or Planning Board	Village Board of Trustees upon recommendation of Committee	Sites incorporate green infrastructure techniques
	ACTION STEP J	Map and prioritize areas within the Village to consider sidewalk and crosswalk conditions, to enable a pedestrian improvement plan with phases to be considered for funding requests to implement.	Committee or Planning Board	Village Board of Trustees upon recommendation	Funding request submitted

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CHAPTER 8.**VILLAGE VISION, GOALS, STRATEGIES, AND CONSIDERATIONS****VILLAGE OF ALEXANDRIA BAY COMPREHENSIVE PLAN IMPLEMENTATION TABLE (continued)****RECOMMENDED ACTIONS:** (in no particular order, except by priority type)

Check when done	MEDIUM-TERM PRIORITY ACTIONS (within 2 to 4 years after plan adoption)		Action	LEAD RESPONSIBILITY	CRITERIA FOR EVALUATING SUCCESS
	ACTION STEP K	Consider updating the zoning district map to protect existing land uses, encourage compatible uses and priority development types to enhance the Village	Zoning Map amendment	Village Board upon committee recommendation	Law adopted by the Village
	ACTION STEP L	Catalogue and prioritize any potential water and/or sewer needs for possible funding requests.	Committee or Village Engineer	Village Board upon committee recommendation	Funding request submitted
	ACTION STEP M	Create and implement policies to encourage standards for historic character and design for new development in priority areas (such as the Business Zone along James, Church and Market Streets).	Zoning Text amendment	Village Board upon committee recommendation	Law adopted by the Village
	ACTION STEP N	Consider a municipal wayfinding signage program or system to encourage use of Village parking lots, the riverwalk and other assets	Village sign committee	Village Board upon committee recommendation	New signage, increased use of Village assets
	ACTION STEP O	Consider zoning district and zoning law amendments to incorporate pedestrian friendly site layouts and land uses types to enhance walkability within the Village as new developments occur.	Zoning Text amendment	Village Board upon committee recommendation	Law adopted by the Village
	ACTION STEP P	Examine options for publicity regarding waterfront resources in the Village, could be included within the wayfinding signage action N.	Committee or Village Board	Village Board upon committee recommendation	Local Chamber or Village Board Implementation
	ACTION STEP Q	Identify historic structures and landmarks to be incorporated in the SEQRA and/or site plan review processes.	Planning Board, Committee or Village Board	Planning Board upon recommendation	Law adopted by the Village
	ACTION STEP R	Address off-premise signage to incorporate Village priorities protecting neighborhood and community character.	Zoning Text amendment	Village Board upon committee recommendation	Law adopted by the Village
	ACTION STEP S	Clarify and simplify the process by which existing small businesses can expand in sites zoned for such uses.	Zoning Text amendment	Village Board upon committee recommendation	Law adopted by the Village

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VILLAGE OF ALEXANDRIA BAY COMPREHENSIVE PLAN IMPLEMENTATION TABLE (continued)

RECOMMENDED ACTIONS: (in no particular order, except by priority type)

Check when done	LONG TERM PRIORITY ACTIONS (within 3 - 5 years after plan adoption)		Action	LEAD RESPONSIBILITY	CRITERIA FOR EVALUATING SUCCESS
	ACTION STEP T	Pursue avenues to address property maintenance, building safety and beautification (elimination of junk and clutter storage) to maintain or enhance neighborhood quality of life.	Committee, Zoning Officer or Village Board	Village Board upon committee recommendation	Law or initiatives adopted by Village Board
	ACTION STEP U	Examine existing and potential sidewalk, riverwalk, pedestrian\bike trail and park or recreation areas throughout the Village, to identify gaps, possible future projects and related needs in the community.	Committee or Village Engineer	Village Board upon committee recommendation	Funding request submitted
	ACTION STEP V	Create an implementation plan with timeframes partnering with the Town of Alexandria and the Village of Alexandria Bay for pedestrian\bicycle improvements connecting nearby Town assets to the Village.	Committee of Town and Village members	Respective Town and Village Boards upon committee recommendation	Funding request submitted